

**English Crossing Pre-Application**

Site Address: **17406 19<sup>th</sup> Ave NE, Marysville, WA 98271**

Parcel Numbers: **310519-004-012-00**

Zoning: **Mixed Use**

Designation: **Mixed Use**

Shoreline: **NA**

Flood Plain Designation: **NA**

Area: **19.52 acres**

**Section 19, Township 31N, Range 05 East**

## **Narrative Site Plan Application**

**Prologue:** We want to clarify upfront that the final boundaries of this site are in-flux. This property has volunteered to allow the roundabout at 172<sup>nd</sup> and 19<sup>th</sup> to shift northwest onto this property to avoid the Ziplly site and the substation. Trying to develop the roundabout in the standard location would require Ziplly to relocate the substation. This would add substantial costs and time to the needed improvements on 172<sup>nd</sup>. It benefits the public at large if 172<sup>nd</sup> is improved to its final design as soon as is possible—*says one that drives it every day*. Other landowners and immediate projects stand to benefit also. The Lodge Apartments and Intracorp both have conditions on their approvals that will limit their yields if this roundabout is not built.

The “flux” is that the City has tasked the Lodge Apartments with the final engineering of the roundabout, which seems appropriate since their project is conditioned, in part, on this roundabout being constructed and they are the closest project to construction. We had submitted a proposed design of a roundabout that provided the benefits (missing Ziplly substation) of moving the location partly onto this property. That proposed design required about a third of an acre of extra dedication. The “Lodge” design, recently submitted, seems to overly burden this property and would require about three quarters of an acre of extra dedication.

English Crossing is committed to dedicating the property needed to build the roundabout without disturbing the Ziplly substation but it does want to see an efficient design that does not take more property than is needed.

This extra dedication over the design we originally offered, would require the loss of about 10 units and loss of Open Space. For some reason the criterion for Open Space with Townhomes in the MU zone is 30% which results in twice the Open Space of a PRD and more than double the Open Space created in apartment projects with twice as many living units.

How this roundabout design sorts out will affect the final design of this project. We would certainly like to open the dialogue on an efficient resolution to this issue as soon as possible.

At the end of the narrative is a brief discussion on 19<sup>th</sup> Ave NE Road Section and the onsite Drive Aisle Sections. We will be asking for small modifications to these standards in further discussions.

**Summary:** The site previously known as “Hanson Sisters” property will now be referred to in this application as English Crossing. It is a 19.52-acre site in the Lakewood Neighborhood Subarea of Marysville. It is zoned Mixed Use (MU). It is located on the northwest corner of 19<sup>th</sup> Ave NE and 172<sup>nd</sup> St NE. The property is bordered on the west by the Burlington Northern Railroad, on the east by 19<sup>th</sup> Ave NE. To the south is 172<sup>nd</sup> ST NE and the north property line coincides with the City Limits. Properties north of this project are in Snohomish County and zoned Rural 5-acre.

Several development scenarios were evaluated for this project but after meeting with the City on those options, the Applicant decided that Townhomes would best suit their development goals. The MU zoning could allow up to 547 living units on this site but the proposal is for 247 Townhomes. The Townhomes will be Condominiums. It would have been preferred to build a “Fee Simple” townhome product but the punitive criteria in the code to get to Fee Simple is too onerous and it cannot be economically justified.

Access will be taken from 19<sup>th</sup> Ave NE across from the existing intersection of 174<sup>th</sup> St NE coming out of the Lodge Apartments. A second access will be toward the north end of the project at what would be “176<sup>th</sup>”. We may have an access offset issue to resolve with our proposed drive aisle and the “176<sup>th</sup> Ave NE alignment east of 19<sup>th</sup> Ave NE. All interior roads will be private drive aisles.

To get separation from the high-water table, several feet of clean structural soils have been placed on the site and will be graded out to accommodate an approved project. Stormwater management will use biocells for treatment and infiltration to the fullest extent possible. Given the soils on this site, there will likely be a combination of infiltration and detention to provide flow control. There will always be enhanced treatment for runoff from pollutant generating surfaces provided by the treatment capability of the biocells.

Portions of some biocells will provide for active recreation with surfaces prepared for active use.

This proposal does anticipate moving the Roundabout at 19<sup>th</sup> and 172<sup>nd</sup> onto this property to facilitate the construction of such roundabout without having to relocate the Zipleby substation on the SE corner of this intersection. It is thought that this dedication would be this project’s contribution to the roundabout installation and others such as The Lodge Apartments and Intracorp (and others) would be responsible for the actual installation. Currently, there is concern over the roundabout design recently submitted by the Lodge and an earlier design we proposed in earlier discussions.

It appears that the trunk line sewer is to be started on the Terra Firma property this Spring (2023). Huseby Homes will extend this main to the SW corner of the Intracorp property for the immediate benefit of the second phase of Vaness. Intracorp should extend this main to 172<sup>nd</sup> but it does appear they are to only extend this to the SW corner of the Zipleby property. This English Crossing project would not likely get into construction until 2024.

**A Little Bit of History:** Land Technologies actually started working on these combined projects in the Lakewood Neighborhood in 2016. Our early tasks were to help get sewer to the property from 156<sup>th</sup> St NE which was challenging due to an uncooperative property owner between these properties

and 156<sup>th</sup>. It has taken ‘seven years’ to finally get some cooperation on that task and that sewer is intended for installation Spring 2023.

We had also worked with the Executives office and Senior Staff to modify the PRD Code to allow for certain code changes that would allow fee-simple townhomes to be built that would be economically feasible. We were close to getting modifications made, but this was stopped as the Council voiced strong opposition to any more “multi-family” developments. The needed Code Modifications were dropped and all plans for fee-simple townhomes on this project were dropped also. Townhomes are not economically feasible using Public Road Standards for access, the extra costs diminish the economic benefit of Townhomes versus Single Family Homes. As housing is getting more expensive, we would hope to revisit this issue and hopefully the Council would be more receptive.

**Phasing:** Due to the size and current absorption rates of homes in this area, the project is anticipated to be divided into 5 phases of about 50 homes each. Each phase will likely take about 1 year to cycle through. It is not a fixed time just estimated timelines that will be affected by Market conditions.

**Bulk Matrix:** Density and Dimensional Criteria:

1. Dwelling Units Per Acre in the Mixed-Use zone is 28. Twenty-eight times the Gross Acres of 19.52 is 547 maximum residential units. Proposal is for 247 single-family attached units.
2. Street setbacks are 20 feet for multi-family structures in the MU zone. Homes in this project do not take access to any public roads but does provide 20’ setbacks from the Drive Aisles to allow for parking in driveways. Some of the homes “back-up” to Public ROW and setbacks do allow for a landscape screening buffer and some SWM, primarily along 19<sup>th</sup> Ave NE, along the public roads. Setbacks from are mostly 25’.
3. Buildings will have 10-foot separation. To allow for a landscape screening buffer and some SWM along 19<sup>th</sup> Ave NE, the homes along the public roads
4. Interior setbacks are “none” except multi-family structures when adjoining land is single-family residential designated property. These Townhomes do provide a minimum 15’ setback from all property lines.
5. Total Impervious Surfaces with roofs, driveways, and roads is less than 60% and will be kept under the 85% Maximum that is allowed.
6. Base Height in MU is 45 feet—proposed townhomes will not exceed that height.
7. Driveways are minimum 20 feet and we will provide three parking stalls per unit in most building configurations. One building type will only provide 2 spaces in the Unit area.

**Consistency Summary with *Lakewood Neighborhood Master Plan Guidelines (LNMP)*:** this section will provide a short summary showing this proposal will be consistent with the LNMPs that apply to this proposal.

**B.3(2)**—Buildings along the street edge, especially, 19<sup>th</sup> Ave NE will have attractive street edge. There will be the standard 5’ planter and behind the ROW will be a 17’ wide Landscaped buffer and bio-cell. Homes will be elevated 4’ to 6’ above the street with a 6’ retaining wall along the back of the landscape buffer. The Townhomes will be offset from top of the retaining wall 8 to 10 feet. The Units facing the street will have elements to provide architectural relief including at least 15 percent of the façade facing the street having transparent windows and doors.

The building elements will be detailed Architectural Plans and elevations will be provided with second review.

**B.3(2)(e)(i)**—Units backing up to open space will have 200 sf of Private Space with at least a 60 sf porch. There will be a solid fence 4’ high with a 2’ lattice on top so homeowners within can see out but will provide some privacy to distant outside views.

**B.3(6)**—Human Scale Standards Final Architectural design will incorporate at least 4 of the 10 listed elements. *My one comment is that homes have been built for humans at “Human Scale” for centuries, where did this vernacular come from? There is a lot of specific detail in this section that is typical to just building a home.*

Other Building Design Elements list in B.3(7) to (13) will be detailed by the Architect with 2<sup>nd</sup> review submittal.

**B.4**—Vehicular access to the buildings will on Drive Aisles facing each unit. Each unit will have a garage on 20’ driveway to provide most resident parking inside the garage and some guest parking in the driveways. There are numerous pocket parking stalls interspersed throughout the site.

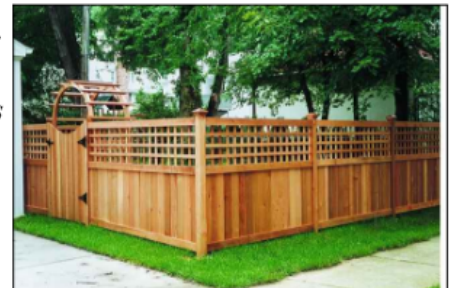
**B.7**—See “Open Space” discussion below.

**B.10**—Solid waste and recycling will be by individual totes in each unit.

**B.11**—Fences will vary throughout the site based on what “service” they will provide. The typical fence will provide for privacy while allowing for close inside views out and obscuring outsiders from seeing in, especially from any distance. The typical fence will be 3’ to 4’ solid cedar with a 2’ lattice on top.



*Fences along lots that back up to Open Space will have 3 to 4 feet of solid board with 2 to 3 feet of open lattice above the solid fence. This is to allow for surveillance of Open Space Residents in the Community. Examples shown are concept, specifications will come with Engineering Plan submittals.*



**B.12**—As a condominium project, the site is served by private Drive Aisles. This site is blocked off on three sides—to the north is rural lands in Snohomish County, to the west is the BNRR Tracks, and to the south is the future roundabout. Only access is from 19<sup>th</sup> Ave NE. We propose a main Boulevard entry at 174<sup>th</sup> St NE that is connected to 19<sup>th</sup> Ave NE, a second connection to 19<sup>th</sup> is proposed about 500’ north of the main boulevard entry.

The interior Drive Aisles are interconnected and taking the form of the trapezoidal shape of the property.

**B.13**—All service and mechanical equipment will be housed inside each unit.

**Housing Mix:** All the proposed homes in this proposal are attached single family units. The units will be three and four-plexes built as condominiums. There will be a mix of architectural units that will provide a range of prices.

To the west and over the RR Tracks, is Mixed Use Property that has a pre-application submitted for Apartments with some Commercial Use along 172<sup>nd</sup>. To the East is The Lodge Apartments. To the south is also Mixed Use and there is an Apartment project started on those sites. The city limit is located along the north property line in rural Snohomish County. The land is zoned R-5 and has single-family homes on large parcels.

**Basic Yield:** The Gross Site Area is 19.52 and yields are based on 28 units times the gross acre. Maximum yield for living units would be 547 units. This project is proposing 247 units.

**Open Space:** This is a proposed Townhouse Development in the Mixed Use (MU) Zone. Per 122C.020.270 Open Space in the Multifamily residential zone is to be 20 percent of the buildings gross floor area. As an apartment complex with 547 units at about 1,000 sf each would require about 2.5 acres of Open Space.

As a PRD, 15% of the Net Project Area would be the required Open Space or about 2.25 acres.

For some reason MMC 22C.020.080 would add 200 sf per unit as Private Open Space and would require 20 percent of the gross floor area to be Open Space or just over 2 acres. However, Section B7 of the Lakewood Neighborhood Master Plan specifically calls out Townhouse Open Space to be 30% of the total development. This Townhouse Development would require 5.86 acres of open space, more than double any other development option including 547 units as apartments. The dynamic of this seems out of sync with the rest of Open Space Requirements in all other zones.

This project, as designed, provides 28.2 percent landscaped Open Space in Common Areas and 1.13 acres of private open space (200+ sf per unit). This project would be dedicating about 0.7 acres to move the roundabout away from the Ziplly Fiber property, based on the design submitted by the Lodge Apartments. An efficient design of the roundabout is needed that first, removes the need to move the Ziplly substation and second burdens this site with an appropriate commitment to dedicate property.

The design of the roundabout we had proposed met these criteria while adding 0.3 acres of dedication. We need to be involved in the discussions on the final design as we are the ones dedicating the extra property to make this roundabout feasible.

If the above points on the Open Space requirements for Townhomes does not gain support for 1.8 percent shortfall, we would like to discuss using MMC 22C.020.300 *On-site recreation—Fee in lieu of open space or recreation space* to close the small gap.

This project is providing the 28.2% Open Space spread throughout the site. There are Active Recreation Areas for Field Spots, Net Sports, Lawn Games, and Children's Play Equipment.

The whole site is "flat" so grade and surface are suitable for recreation.

Each unit will have minimum of 200 sf of Private Space out the back of each unit.



**Parking:** The Townhome project requires two parking spaces per unit/residence with 1 Guest Spot per unit/residence. There are 247 proposed units that will have two-car garages, some will be tandem but most are side by side parking. Two spaces in the garage of the side-by-side units and one in the Tandem Garages (30 units) will provide 464 spaces. There are 299 Guest Spaces provided in pocket parking stalls or as tandem spaces on driveways. Thirty resident spaces will be provided in pocket parking next to the buildings.

A Total of 731 spaces are required, 793 spaces are proposed.

**Access and Frontages:** Access to the site is off 19<sup>th</sup> Ave NE, a public road. The LNMP designates this road to have a turn lane and bike lanes. This project needs to dedicate 15' to provide 35' half ROW versus 32.5' to make up for the under-dedicated ROW on the Lodge Side. The road section on the Lodge Side did only dedicate 30.5. To help adjust to this under-dedication, the road section has been adjusted to eliminate the planter and put a 6' sidewalk against the curb is to be allowed. The adjustments will put the center of the road 3.5' offset from the center of ROW.

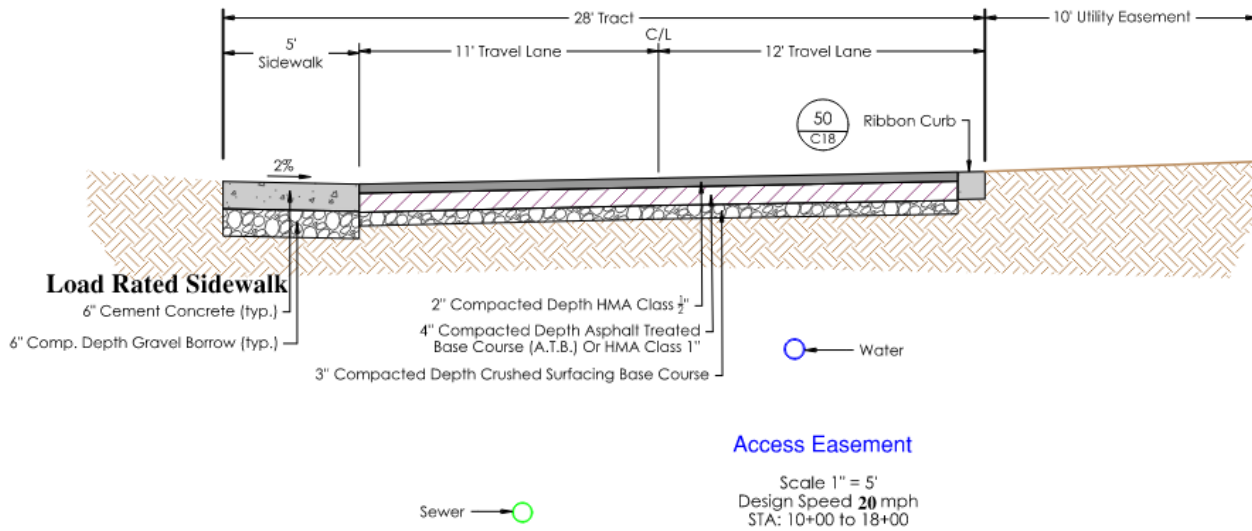
We are glad to accommodate to make the road system consistent with Comprehensive Plans. One little issue that will need further attention on this is SWM. The existing road and roadside ditches are very flat and have little rise to the discharge points at 172<sup>nd</sup>. The outlet inverts at 172<sup>nd</sup> do not offer much drop and the groundwater elevations in winter are maybe 2' below the existing ground. This existing condition makes it very difficult to provide a standard solution to the runoff from 19<sup>th</sup> AVE NE.

While not perfect, we have proposed a Bio-cell Swale outside the ROW. This will require slot-drains in the sidewalk as CBS and pipes would end up being too deep to stay above the fixed inverts at 172<sup>nd</sup>. We will need to discuss this approach to SWM given the difficult existing conditions.

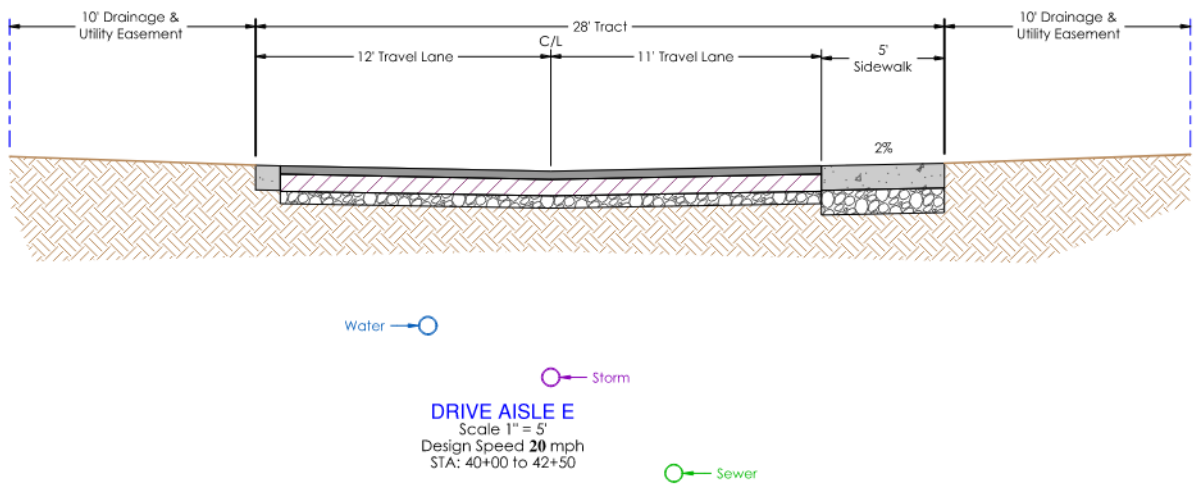
Interior access aisles are private Drive Aisles. We find no Standard Drawings defining what the City wants to see in a Drive Aisle. We have proposed an Aisle Section that we have successfully used in many other jurisdictions. This section consists of 22' of pavement, a 1' wide Concrete Ribbon with WSDOT specs, and a 6" deep Concrete Sidewalk on one side capable of supporting a fire truck. This



provides a 28' wide road and pedestrian corridor. Due to high water table and restricted elevations of the offsite discharge point, the sidewalk is sloped into the Drive Aisle and the Paved Travel lanes shed toward the sidewalk. This counter slope of sidewalk and Drive Aisle form the trough to move stormwater runoff flows. It is critical that the stormwater flows be conveyed at the surface as much as possible to work with the discharge elevations from the site and to keep the Treatment bio-cells as shallow as feasible.



Some lengths of Drive Aisle do have inverted crowns:



We believe there will be some required discussions on these issues on Access and Frontages.

## PROJECT DESCRIPTION

**Existing Site Use:** the existing site is one parcel. It had one rural farm residence and barn; those are gone now. The site currently has stockpiled structural fill in anticipation of development.

There has been a wetland review of the site and no critical areas were identified on the site.

**Proposed Site Use:** The proposed use of the site will be for 247 townhome units developed as condominiums.

**Zoning/Land Use:** The zoning is Mixed Use (MU). Surrounding uses are consistent with this zoning.

**Stormwater Management/LID:** The site will use a combination of infiltration and detention for the development using Biocell Swales and Bio-infiltration cell for Stormwater Management. Soils do not support one hundred percent infiltration and detention will be required. Detention will be in rock chambers that will allow as much infiltration as is feasible. The structures and conveyance depths are limited by the discharge elevations at 172<sup>nd</sup>.

## **SITE DESCRIPTION**

### **Soils**

Primary soils are mapped by the Natural Resources Conservation Service (NRCS) as Custer fine sandy loam is a small area at the southeast corner of the site. Most of the site is mapped Lynnwood loamy sand.

### **Drainage**

The land is part of the outwash plains and has little slope. There are drainage ditches along 19<sup>th</sup>. These have been determined by Fisheries not to be fish streams and the Army Corp has determined the ditches are non-jurisdictional.

### **Topography**

The site is on the edge of the outwash plans and rises to the west with a slight southerly slope toward 172<sup>nd</sup>.

### **Wetlands**

There are no wetlands on site as verified by earlier reviews.

### **Streams**

No streams on site. The non-fishery ditches do discharge to a Fish Stream about 3,000 feet south of this property.

### **Vegetation**

The site had been a small farm raising some cattle and grass hay. The whole site was covered with the remnant grasses from the farm.

### **Wildlife**

Only common wildlife found in the area; no protected species known to be on or near the site.

### **Surrounding Land Use**

To the east is the Lodge Apartments. Another apartment project by Intracorp is in construction to the southeast across 172<sup>nd</sup>. Railroad tracks to the west and west of the tracks another apartment project has been submitted for a Pre-application review. The City limits are to the north with Rural 5-acre zoning in Snohomish County.

### **Traffic Patterns**



Existing access will be from 19<sup>th</sup> Ave NE at 174<sup>th</sup> St NE. Most traffic will exit south to 172<sup>nd</sup> and likely to the freeway. See Traffic Report for details.

### **Nearest Schools**

Lakewood Schools are about ¼ mile west of 19th Ave roundabout. Lakewood schools bus all students to school. There is a delineated path along the south side of 172<sup>nd</sup> and a 10' sidewalk will be built on the north side as properties develop along 172<sup>nd</sup>.

### **Bus Stops**

Bus stops are on 172<sup>nd</sup> St NE just east of the project.

### **Walkways**

There is a designated walkway along the north side of 172<sup>nd</sup> and sidewalks are being developed all along 172<sup>nd</sup> as property develops.

### **Other Unique Characteristics of Site**

There are some unique circumstances with this site and project that will require further discussions to arrive at a feasible resolution.

1. Roundabout Final Design as it impacts property and Open Space. The recently submitted design consumes .7 acres of this site. The concept design we submitted when volunteering to contribute ROW, only consumed .3 acres. The Lodge Apartment design has not had any consideration to impacts to this property. This Applicant is committed to providing the space needed to build this roundabout without having to move the structures on their site—but we would expect to see a reasonable design that still respects this Applicant's property.
2. 19<sup>th</sup> Ave NE frontage improvements and SWM along that frontage require innovative solutions. The solution to the issue of the short-sided ROW on the Lodge side seems reasonable but the management of stormwater is complicated. The road is flat and the discharge elevations at 172<sup>nd</sup> are restrictive. We are proposing a 15' wide bio-swale that will need slot drains through the sidewalk to move stormwater along at shallow enough depths to stay above the discharge invert at 172<sup>nd</sup> St NE.
3. Onsite Drive Aisle Sections will need to be discussed with Public Works and Fire. We note the City does not seem to have a published Aisle Section (at least that we could find in the Design Manual). We offered what we commonly use in our Fee Simple Townhome projects in other jurisdictions. What we are proposing does not exactly coincide with some comments made in the Pre-application review.

Thank you

Merle Ash  
Land Technologies Inc.